

DRAFT

ADA TRANSITION PLAN

Americans with Disabilities Act Transition Plan:
Pedestrian Facilities in the City Of Thousand
Oaks Street Right-of-Way
For Curb Ramps

City of

Thousand Oaks

September 2016

ADA TRANSITION PLAN FOR THE CITY OF Thousand Oaks

The plan contained herein has been prepared by, or under the direct supervision of the following registered Civil Engineer:

Clifford G Finley
City Engineer
City of Thousand Oaks

(Seal)

ADA TRANSITION PLAN FOR THE CITY OF Thousand Oaks

Table of Contents

Contents

I. INTRODUCTION 3

II. BACKGROUND 3

 Legal Requirements 3

 Purpose of the ADA Transition Plan..... 3

 Overview of the Americans with Disabilities Act (ADA)..... 4

III. DEVELOPMENT OF ADA TRANSITION PLAN 6

 Identified Obstacles to the Public Right-of-Way..... 6

IV. METHODOLOGY AND APPROACH 7

Appendix A
Sample Survey/Evaluation Sheet

Appendix B
Curb Ramp Standard Drawings

Appendix C
2010 Curb Ramp ADA compliant Program

Appendix D
2014 Curb Ramp ADA compliant Program

Appendix E
ADA Transition Plan Summary
Detailed ADA Transition Plan

I. INTRODUCTION

The City of Thousand Oaks recognizes that its programs, services and activities are essential to maintain the public good, welfare and safety. The City continues to respond to the needs and rights of people with disabilities recognized by the American with Disabilities Act (ADA). To ensure that all of its citizens and the general public have the opportunity to participate in the ADA program, the City has been actively making necessary changes to implement its ADA program to improve accessibility in the public right-of-way. This ADA transition plan applies to facilities located in the City of Thousand Oak's street Right Of Way.

II. BACKGROUND

Legal Requirements

The federal legislation known as the American with Disabilities Act (ADA), enacted on July 26, 1990, provides comprehensive civil rights protections to persons with disabilities in the areas of employment, state and local government services, and access to public accommodations, transportation, and telecommunications. Title II specifically applies to "public entities" (state and local governments) and the programs, services, and activities they deliver. Title II Article 8, requires public entities to take several steps designed to achieve compliance, the plan shall, at a minimum include:

1. A list of the physical barriers in a public entity's facilities that limit the accessibility of its programs, activities, or services to individuals with disabilities.
2. A detailed outline of the methods to be utilized to remove these barriers and make the facilities accessible.
3. The schedule for taking the necessary steps to achieve compliance with Title II.
4. The name of the official responsible for the plan's implementation.

Transition plans provide a method for a public entity to schedule and implement ADA required improvements to existing streets. Before a transition plan can be developed, an inventory of the current curb ramps must be developed.

Purpose of the ADA Transition Plan

The ADA Standards as they are appended in Title 28 of the Code of Federal Regulations, part 36 (from hereon referred to as the Americans with Disabilities Act Accessibility Guidelines or ADAAG), and the California State Accessibility Standards (from hereon referred to as the CBC, CSAS) as they are contained in the current California Building Code. This document identifies, locates, and cites specific codes for architectural barriers in both the federal and state accessibility guidelines outlined in the ADAAG and CBC. The City of Thousand Oaks Transition Plan identifies physical barriers in the streets and pedestrian routes within the public right-of-way, and estimates the costs for their mitigation.

ADA Transition Plan includes an inventory of existing physical barriers and deficiencies within city facilities. (See Table 1)

Table 1

FACILITY	Possible Barriers
Curb Ramp	Missing, Doesn't Fall within Marked Crosswalk, Doesn't Conform to Guidelines
Curb Ramp Flares	Missing Where Required, Too Steep
OBSTRUCTIONS	Signs, Mail Boxes, Fire Hydrants, Benches, Telephones, Traffic Signal Poles, Traffic Signal Controller Boxes, Newspaper Boxes, Drainage Structures, Tree Grates, Pole Mounted Objects, Standing Water

Overview of the Americans with Disabilities Act (ADA)

The ADA prohibits discrimination on the basis of disability in employment, state and local governments, public accommodations, commercial facilities, transportation, and telecommunications. It also applies to the United States Congress. ADA has 5 titles or sections that are briefly described on the following page.

To be protected by the ADA, one must have a disability or have a relationship or association with an individual with a disability. An individual with a disability is defined by the ADA as a person who has a physical or mental impairment that substantially limits one or more major life activities, a person who has a history or record of such impairment, or a person who is perceived by others as having such impairment. The ADA does not specifically name all of the impairments that are covered.

ADA Title I: Employment

Title I requires employers with 15 or more employees to provide qualified individuals with disabilities an equal opportunity to benefit from the full range of employment-related opportunities available to others. In the City of Thousand Oaks, Human Resources department handles all matters pertaining to ADA Title I.

ADA Title II: State and Local Government Activities

Title II covers all activities of state and local governments regardless of the government entity's size or receipt of federal funding. Title II requires that state and local governments give people with disabilities an equal opportunity to benefit from all of their programs, services, and activities (e.g. public education, employment, transportation, recreation, health care, social services, courts, voting, and City meetings). ADA Title II is the focus for this Transition Plan.

State and local governments are required to follow specific architectural standards in the new construction and alteration of their buildings. They also must relocate programs or otherwise provide access in inaccessible older buildings, and communicate effectively with people who have hearing, vision, or speech disabilities. Public entities are not required to take actions that would result in undue financial and administrative burdens. They are required to make reasonable modifications to policies, practices, and procedures where necessary to avoid discrimination, unless they can demonstrate that doing so would fundamentally alter the nature of the service, program, or activity being provided.

ADA Title III: Public Accommodations

Title III covers businesses and nonprofit service providers that are public accommodations, privately operated entities offering certain types of courses and examinations, privately operated transportation, and commercial facilities. Public accommodations are private entities who own, lease, lease to, or operate facilities such as restaurants, retail stores, hotels, movie theaters, private schools, convention centers,

doctors' offices, homeless shelters, transportation depots, zoos, funeral homes, day care centers, and recreation facilities including sports stadiums and fitness clubs. ADA Title III does not apply to this Transition Plan.

ADA Title IV: Telecommunications Relay Services

Title IV addresses telephone and television access for people with hearing and speech disabilities. It requires common carriers (telephone companies) to establish interstate and intrastate telecommunications relay services (TRS) 24 hours a day, 7 days a week. ADA Title IV does not apply to this Transition Plan.

ADA Title V: Miscellaneous Provisions

Title V clarifies that both states and Congress are covered by all provisions of the ADA. It also provides for recovery of legal fees for successful proceedings pursuant to the Act and establishes a mechanism for technical assistance along with specific instructions to many federal agencies required to implement the Act. Additionally, Title V includes a provision prohibiting either (a) coercing or threatening or (b) retaliating against the disabled or those attempting to aid people with disabilities in asserting their rights under the ADA. ADA Title V does not apply to this Transition Plan.

III. DEVELOPMENT OF ADA TRANSITION PLAN

The ADA Transition Plan documents the access barriers in streets within the public right-of-way. The documented access barriers indicate that existing conditions of these City facilities deviate from current State and Federal accessibility standards for new construction. For each identified barrier, the ADA Transition Plan cites the code sections and requirements from the ADA Accessibility Guidelines (ADAAG) as well as Title 24 of the California State Accessibility Standards (CSAS).

Identified Obstacles to the Public Right-of-Way

Preliminary Evaluation

The first tier is a Preliminary Evaluation of the intersections. The purpose of this evaluation is to determine which intersections are non-compliant in accordance with the ADA Guidelines and to develop a comprehensive overview of the complete pedestrian network. The preliminary inventory evaluates two (2) criteria for curb ramps:

1. Is there a curb ramp?
2. Does the curb ramp have a color contrasting detectable warning?

The Preliminary Evaluation utilizes aerial and street-level photography to view each intersection. The above criteria can be observed on the aerials and are key design components to determine ADA compliance. If the curb ramps do not meet the criteria, then that intersection is obviously non-compliant with the ADA Guidelines. If it did meet the criteria, then that intersection would be “potentially compliant” and would need a detailed evaluation to determine if it fully complies with the ADA Guideline.

Detailed Evaluation

The second tier is a detailed evaluation of the intersections identified as “potentially compliant” during the Preliminary Evaluation. This requires fieldwork at the intersection and measuring of specific physical attributes, such as width, running slope, to determine compliance to the identified ADA barriers. When the data is gathered, it is recorded into a Curb Ramp database (Which is called inventory). The result from this evaluation is a detailed understanding of the ADA barriers at that intersection. The following steps are taken:

- Field Survey
- Assessment
- Prioritization

Field survey

In order to determine which curb ramps are not in compliance with ADA, the City performed field surveys of the city street network scheduled for pavement rehabilitation and substantial resurfacing treatments in accordance with the city Pavement Management Program (PMP) prioritization. The survey fulfills the first requirement for the ADA Transition Plan by identifying physical obstacles limiting the accessibility to facilities in the city’s right of way. The field survey measures all the ADA requirements for each existing curb ramp. A sample of our Survey/Evaluation Sheet is presented in Appendix A. The City’s curb ramp standard drawings are presented in Appendix B.

Assessment

After entry of the field measurements to the city database, curb ramps that need improvement are separated into three groups, NEW CONSTRUCTION, RE-CONSTRUCTION and MODIFICATION.

- New construction: Sidewalk and curb exists but curb ramp is missing. Part of the sidewalk and curb and gutter would be removed and replaced with a new ADA complaint curb ramp.
- Reconstruction: Ramp has one or more slope deficiencies, ramp does not meet width requirement, ramp does not meet length requirement, ramp is not within crosswalk, may need truncated domes, grooved borders and or lip removal. These ramps would be removed and replaced with a new ADA complaint ramp.
- Modification: Ramp meets all ADA slope requirements but lacks truncated domes, grooved borders and may need the gutter lip removed. These ramps would be modified to meet ADA requirements.

Prioritization

Prioritization for the transition plan is based on the Pavement Management Program (PMP) rehabilitation and maintenance prioritization plan. The City of Thousand Oaks has a 10 year PMP identifying streets/areas that will be rehabilitated or resurfaced in the next ten years. Curb ramps in any proposed rehabilitation area will be newly constructed, reconstructed or modified prior to treatments such as overlay, cape seal and micro-surfacing.

IV. METHODOLOGY AND APPROACH

The City of Thousand Oaks has been implementing a strong citywide curb ramp program as part of its capital improvement program for many years. As an example the City completed a major curb ramp program in 2010, installing and reconstructing a total of 363 curb ramps, see Appendix C.

In 2014, the City decided to implement a 30-year ADA transition plan that will install all necessary new curb ramps, as well as reconstructing, or modifying all existing curb ramp to meet the ADA requirement. As Part of the 2014 Curb Ramp program, 531 curb ramps were either Modified, Newly Constructed or Re-constructed to meet the 2010 ADA requirements, see appendix D.

As of January 2016, there are 4,176 existing curb ramps that are cataloged into the city's inventory database. In addition, 571 missing curb ramps will be required to be constructed for a total of 4,747 curb ramps in the city's street network. Based on the PMP prioritizing and projected city's budget, for the next 10 years the city estimates that 1,168 curb ramps will be either modified, reconstructed or newly constructed. The ADA Transition plan assumes 25 percent of existing curb Ramps meet the ADA requirements, 65 percent require reconstruction and 35 percent need modification.

Based on total number of curb ramps in the City network and implementing the initial 10 year plan, the City plans to Modify/Construct/Reconstruct approximately 198 curb ramps from 2024 (Year 10) to 2044 (Year 30) to bring all curb ramps within the city's streets into compliance with the ADA requirements. Details including location, number of curb ramps and overall cost summary for each year of the transition plan can be found in Appendix E.

**City of
Thousand Oaks**

**ADA TRANSITION PLAN
APPENDIX A**

Sample Survey/Evaluation Sheet

AREA 10B - DATA COLLECTION

CURB RAMP #	CURB RAMP PRESENT	WIDTH OF RAMP(IN)	SLOPE OF RAMP (%)	SLOPE OF RAMP FLARE (%)		LANDING SLOPE (%)	LANDING LENGTH	LANDING SIDEWALK APPROACH SLOPE (%)		GUTTER SLOPE (%)	GUTTER LIP	GROOVED BORDER	TRUNCATED DOMES	WITHIN CROSSWALK	COMPLY TO ADA
				L	R			L	R						
4377	Y	42.5	1.1	1.5	0.9					7	N	Y	N	Y	Y
3068	Y	55	0.4	6.8	6.4					5.1	N	Y	N	Y	Y
4280	Y	48	0.7	4.2	4.5					5.6	N	LEFT ONLY	N	Y	Y
4281	Y	48	0.8	1.3	5.8					8.1	N	RIGHT ONLY	N	Y	Y
142	Y	55	1.6	1.3	9.1					12.4	Y 0.5"	Y	N	Y	N
143	Y	53	0.2	10.5	0.9					17	Y 0.5"	Y	N	Y	N
4272	Y	54	3.7	9.0	4.9					18.7	Y 0.5"	Y	N	Y	N
4273	Y	54	1.6	8.3	0.0					11.2	Y 0.5"	Y	N	Y	N
6137	Y	48	13.1	2.6	6.6					2.6	N	Y	N	Y	N
3304	Y	48	13.4	7.8	5.9					1.2	N	Y	N	Y	N
6511	Y	48	4.5	4.2	0.0					0.3	N	LEFT ONLY	Y	Y	Y
3292	Y	48	2.1	8.9	3.5					3.7	N	Y	Y	Y	Y
3295	Y	64	1.6	0.5	11.7					2.3	N	Y	N	Y	Y
5786	Y	65	1.1	7.5	8.9					3.2	N	Y	N	Y	Y
5238	Y	50	0.8	3.7	11.1					7.3	N	Y	N	Y	Y
1306	Y	50	1.3	4.0	5.5					5.6	N	Y	N	Y	Y
4252	Y	48	0.4	7.9	0.2					11.1	Y 0.5"	Y	Y	Y	N
4253	Y	48	2.1	3.6	5.7	1.4	65			14	Y 0.5"	Y	Y	Y	N
6282	Y	48	1.5	4.7	8.2					9.7	N	Y	Y	Y	Y
6278	Y	48	1.7	3.8	7.6					10.9	N	Y	Y	Y	Y
3050	Y	51	2.1	3.4	7.4					7.4	N	RIGHT ONLY	N	Y	Y

UTILITIES	COUNT			NOTES: PLEASE NOTE THAT FOR THIS AREA, THE TOP LANDING WIDTH WAS MEASURED RATHER THAN THE TOP LANDING LENGTH (1) WW MANHOLE WITH CROSSWALK AVE DE ROYALE (2) WW MANHOLES ARE WITHIN FLOWLINE ALMON & QUINTA VISTA
DISTRICT - CITY OF THOUSAND OAKS	AS-BUILT		ACTUAL	
WW MANHOLE	15		IN GRIND ZONE - 8 OUT OF GRIND ZONE - 2	10
SD MANHOLE	8		IN GRIND ZONE - 0 OUT OF GRIND ZONE - 8	8
VALVES	25		IN GRIND ZONE - 0 OUT OF GRIND ZONE -33	33

**VIOLATION
NON-COMPLIANT**

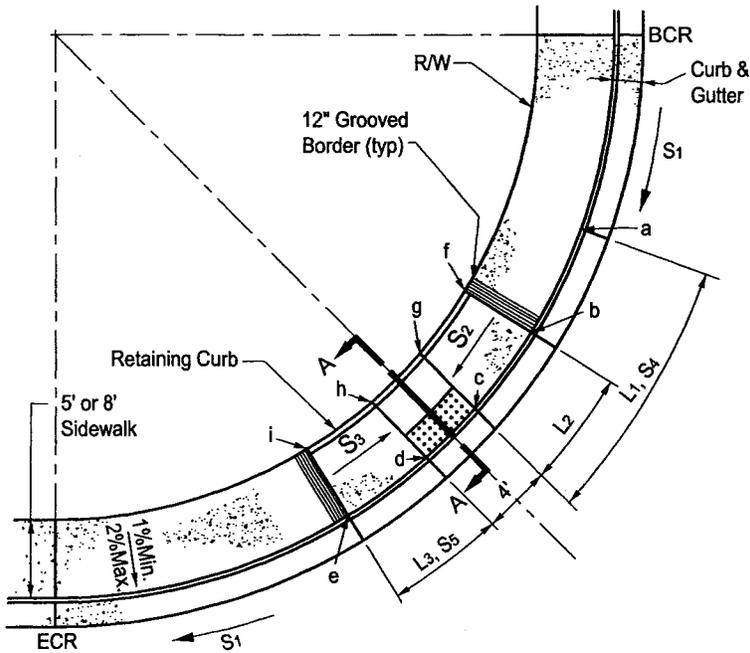
CURB RAMPS NON COMPLIANT	8
CURB RAMPS COMPLIANT	15
TOTAL CURB RAMPS	23

MODIFIED CURB RAMPS	6
RECONSTRUCT CURB RAMPS	2
CONSTRUCT NEW CURB RAMP	0

**City of
Thousand Oaks**

ADA TRANSITION PLAN APPENDIX B

The City's Standard Curb Ramp Drawings



Transition Lengths

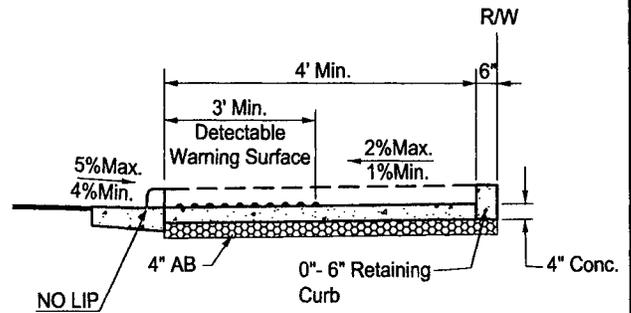
L_1 = gutter transition
 L_2, L_3 = ramp length

Slopes

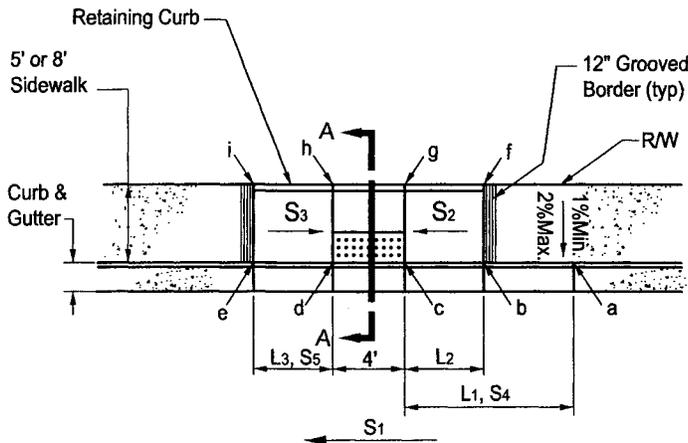
S_1 = F_L slope BCR to ECR
 or street slope
 S_2, S_3 = ramp slope
 S_4, S_5 = F_L slope

Elevations

a, b, c, d, e = TC, F_L , EL
 f, g, h, i = TC, BW



SECTION A-A
 NOT TO SCALE



NOTES:

1. Refer to Plate No. 5-4 for notes and details.
2. All dimensions, elevations, slopes and transition lengths to be designed by Developer's Engineer.

5	Add Detectable Warning Surface	10-22-08	MAP
2	Correct typographical error	12-15-04	JTS
1	4' Landing Dimension	6-26-03	JTS
CHG	DESCRIPTION	DATE	INITIAL

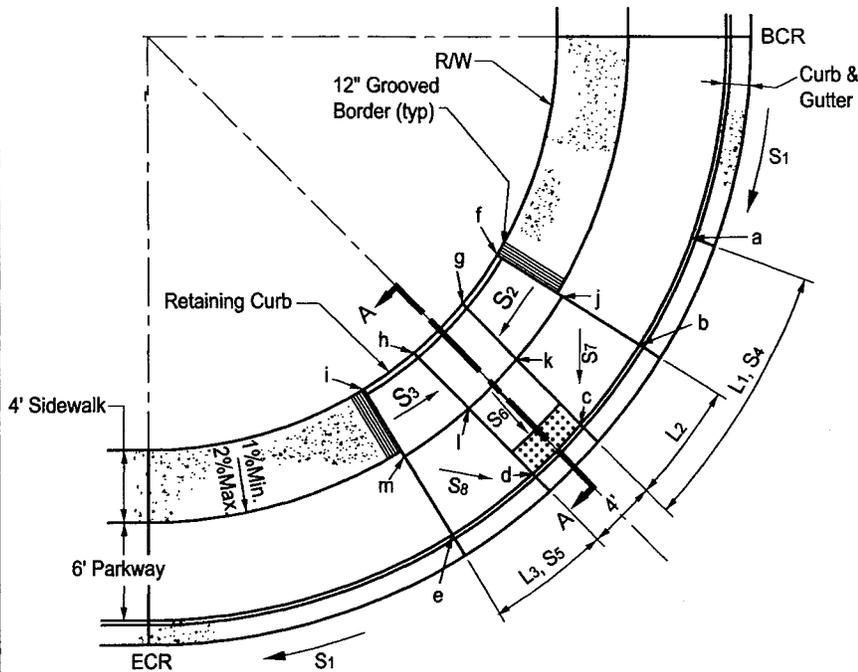
CITY OF THOUSAND OAKS
 PUBLIC WORKS DEPARTMENT

APPROVED

Jeff Burzgin
 CITY ENGINEER
 10/23/08
 DATE

STANDARD
 CURB ACCESS RAMP
 ATTACHED SIDEWALK

PLATE NO.
5-1



Transition Lengths

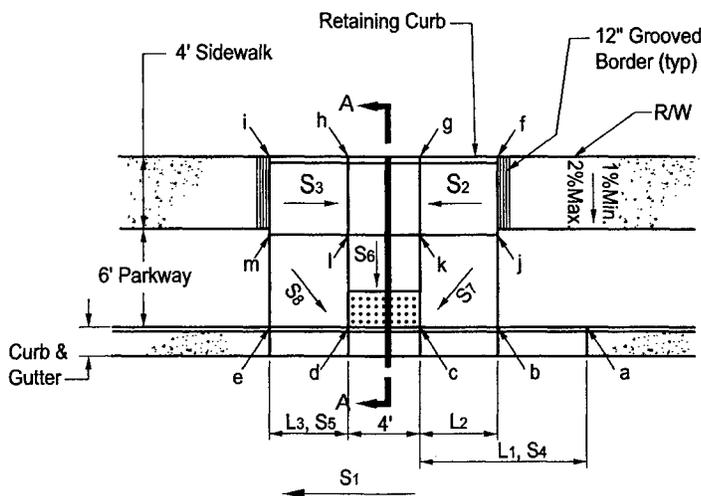
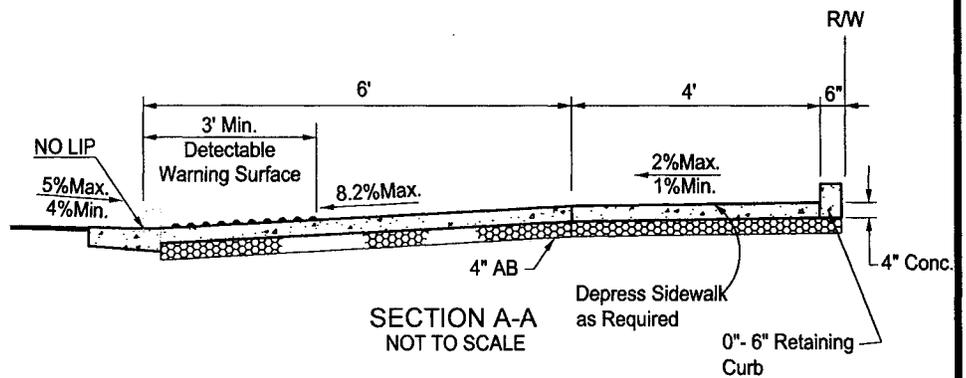
L₁ = gutter transition
 L₂, L₃ = ramp length

Slopes

S₁ = F_L slope BCR to ECR or street slope
 S₂, S₃, S₆ = ramp slope
 S₄, S₅ = F_L slope
 S₇, S₈ = wing slope, varies, 10% max.

Elevations

a, b, c, d, e = TC, F_L , EG
 f, g, h, i = TC, BW
 j, k, l, m = finish surface



NOTES:

1. Refer to Plate No. 5-4 for notes and details.
2. All dimensions, elevations, slopes and transition lengths to be designed by Developer's Engineer.

5	Add Detectable Warning Surface	10-22-08	MAP
4	Correct typographical error	12-15-04	JTS
1	4' Landing dimension	6-26-03	JTS
CHG	DESCRIPTION	DATE	INITIAL

APPROVED

[Signature]
 CITY ENGINEER

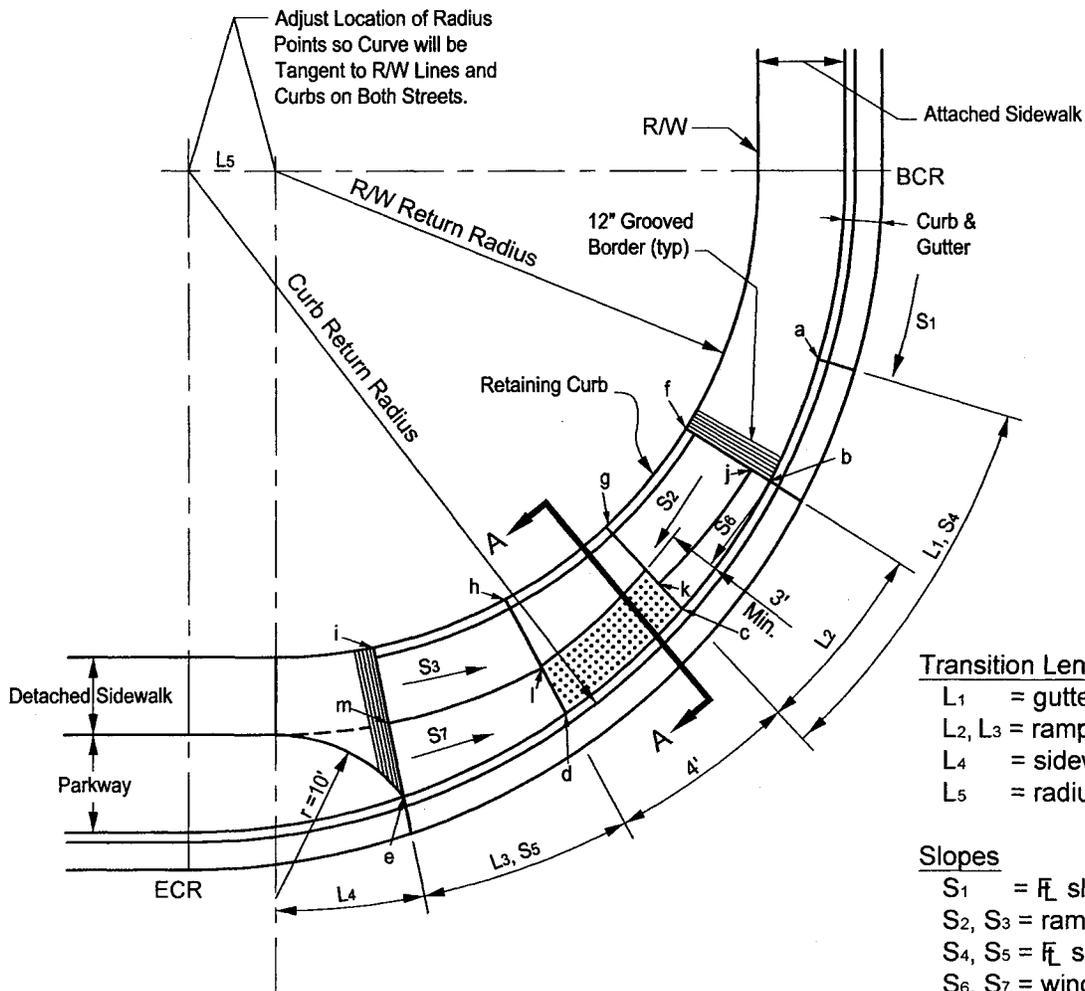
10/23/08
 DATE

CITY OF THOUSAND OAKS
 PUBLIC WORKS DEPARTMENT

STANDARD
 CURB ACCESS RAMP
 DETACHED SIDEWALK

PLATE NO.

5-2



Transition Lengths

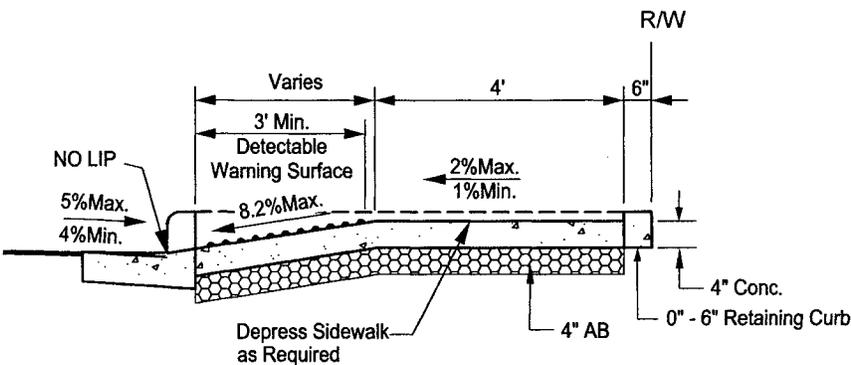
- L₁ = gutter transition
- L₂, L₃ = ramp length
- L₄ = sidewalk transition
- L₅ = radius point offset

Slopes

- S₁ = \bar{f}_L slope BCR to ECR
- S₂, S₃ = ramp slope
- S₄, S₅ = \bar{f}_L slope
- S₆, S₇ = wing slope, varies, 10% max.

Elevations

- a, b, c, d, e = TC, \bar{f}_L , EG
- f, g, h, i = TC, BW
- j, k, l, m = finish surface



SECTION A-A
NOT TO SCALE

NOTES:

1. Refer to Plate No. 5-4 for notes and details.
2. All dimensions, elevations, slopes and transition lengths to be designed by Developer's Engineer.

5	Add Detectable Warning Surface	10-22-08	MAF
4	Correct typographical error	12-15-04	JTS
1	4' Landing dimension	6-26-03	JTS
CHG	DESCRIPTION	DATE	INITIAL

CITY OF THOUSAND OAKS
PUBLIC WORKS DEPARTMENT

APPROVED

Jay Sprygin
CITY ENGINEER
10/23/08
DATE

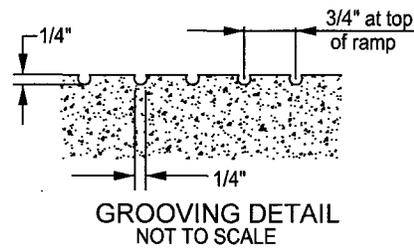
STANDARD
CURB ACCESS RAMP TRANSITION -
ATTACHED TO DETACHED SIDEWALK

PLATE NO.

5-3

NOTES FOR ACCESS RAMP STANDARD PLATE NOS. 5-1, 5-2 AND 5-3

1. Curb access ramps shall be constructed at each corner of street intersections and where a cross walk or pedestrian way crosses a curb. Two ramps, centered on each cross walk, shall be provided at curb returns on primary, secondary, industrial and commercial roads (curb radius of 35'). For 25' radius curb returns, one ramp, centered in the return, shall be provided. Ramps shall not extend beyond the curb return BCR or ECR.
2. Ramps shall be min. 4' wide and shall generally lie in a single slope plane with minimal surface warping. Ramp slope shall be min. 6.7% and max. 8.2%, with cross slope between 1.5% and 2%. Landings shall be 4' by 4' with a max. 2% slope in any direction.
3. The bottom of all ramps or landings shall be flush with the adjacent gutter (NO LIP). The adjacent gutter slope shall be between 4% and 5%.
4. Provide a 12" wide grooved border at the top of each ramp. See detail below for dimensions of grooves. The full width and the lower 3' of ramps or landings shall have a truncated dome detectable warning surface. Domes shall be aligned on a square grid in the predominate direction of travel.
5. The dimensions and spacing of the truncated dome on the detectable warning surface shall be in accordance with ADA regulations and SPPWC Std. Plan 111 (Latest Version).
6. Truncated dome panels shall be inset into the concrete and shall be constructed so as to remain removable/replaceable in-kind with steel bolt fasteners as anchorage. Panels shall not be applied using adhesive nor with permanently embedded anchors. Concrete or brick pavers with precast truncated domes may be used.
7. Color of the detectable warning surface shall contrast visually with adjoining surfaces, either light-on-dark, or dark-on-light. Selection of color shall be approved by the City Engineer prior to installation. Terracotta color is preferred.
8. The Developers Engineer shall design each curb access ramp, including all dimensions, elevations, slopes and transition lengths as shown on Plates 5-1, 5-2 and 5-3. Approximate ramp and gutter/flowline transition lengths for various street slopes are as follows:
 - a. For street slopes <0.6%, L1=7', L2=7', L3=7', hold flowline grade.
 - b. For street slopes 0.6% to 2.5%, L1=10', L2=7', L3=5', hold edge of gutter grade.
 - c. For street slopes >2.5%, L1=15', L2=15', L3=3.5', hold edge of gutter grade.
9. The concrete surface of the entire curb access ramp shall be slip resistant and contrasting from the finish of the adjacent sidewalk.
10. Sawcut existing sidewalk at nearest score line and construct new concrete sidewalk each side of curb access ramp per Plate 8-3. Provide sidewalk extensions as required to maintain 4' min. path of travel per Plate Nos. 8-8 or 8-9.
11. Provide 6" concrete curb at end of ramp where sidewalk does not continue.
12. Sawcut and remove/replace min. 2' of existing AC pavement along entire length of access ramp. Match existing structural section, min. 3" AC over 6" AB.



6	Corrected Detectable Warning Reference	3-27-12	MAF
5	Revised for Detectable Warning Surface	10-22-08	MAF
CHG	DESCRIPTION	DATE	INITIAL
APPROVED	<i>Jay H. Morgan</i> CITY ENGINEER	3.27.12 DATE	

CITY OF THOUSAND OAKS
PUBLIC WORKS DEPARTMENT

STANDARD
CURB ACCESS RAMP NOTES

PLATE NO.
5-4

**City of
Thousand Oaks**

ADA TRANSITION PLAN APPENDIX C

2010 Curb Ramp Program

2010 CURB RAMP ADA COMPLIANT PROGRAM

Legend

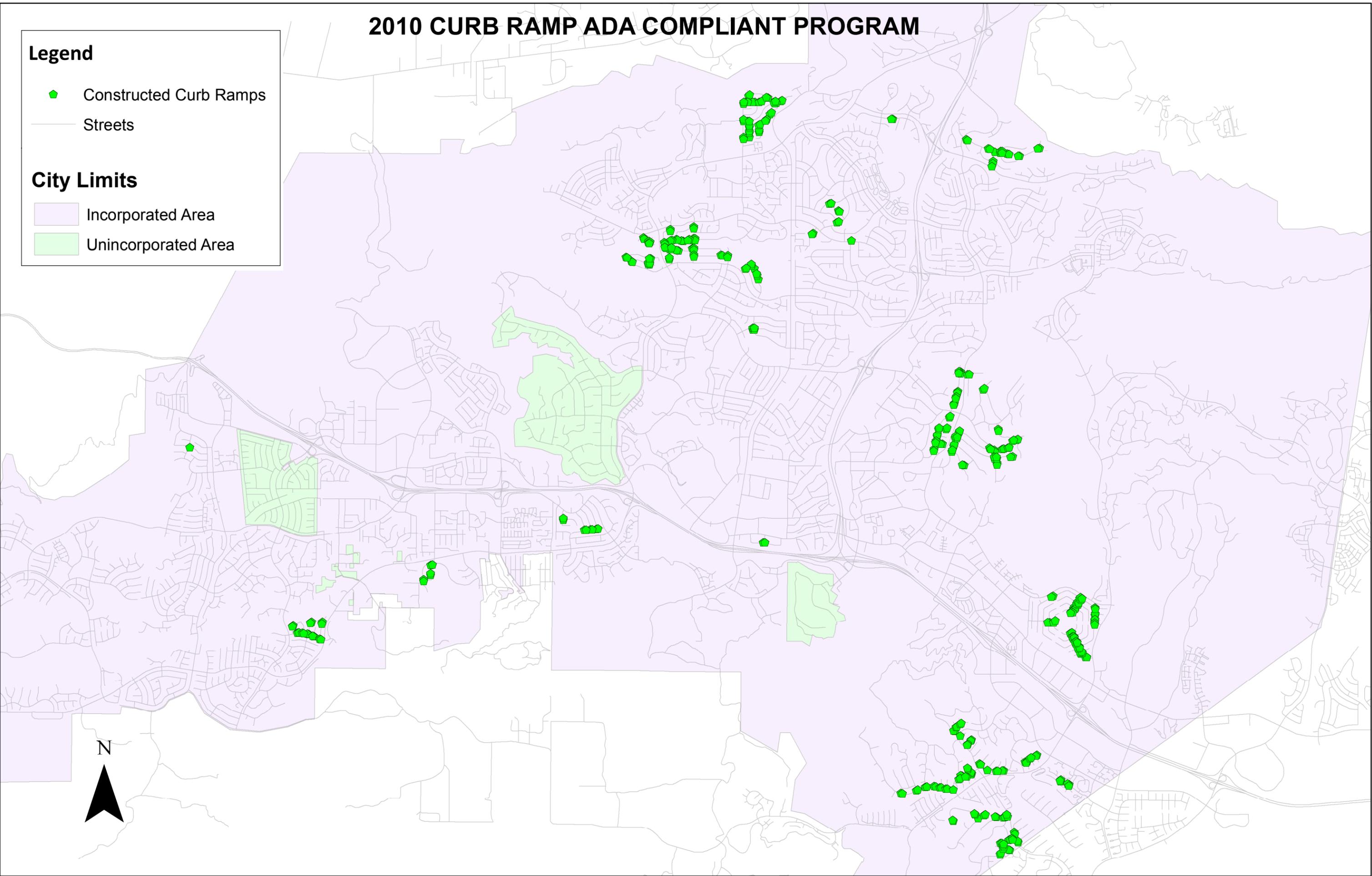
◆ Constructed Curb Ramps

— Streets

City Limits

■ Incorporated Area

■ Unincorporated Area



**City of
Thousand Oaks**

ADA TRANSITION PLAN APPENDIX D

2014 Curb Ramp Program

2014 CURB RAMP ADA COMPLIANT PROGRAM

Legend

▲ Reconstructed Curb Ramps

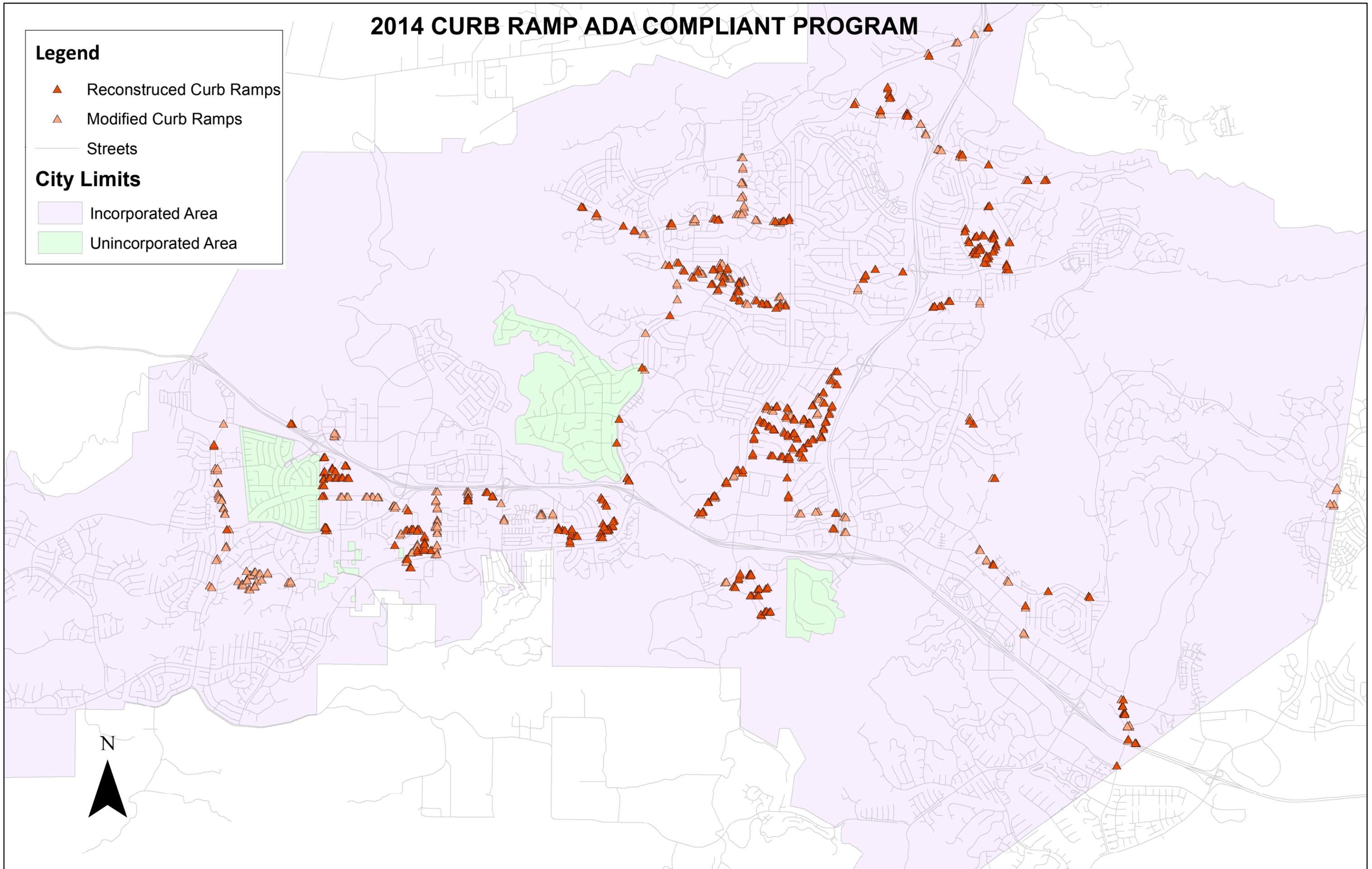
▲ Modified Curb Ramps

— Streets

City Limits

■ Incorporated Area

■ Unincorporated Area



**City of
Thousand Oaks**

ADA TRANSITION PLAN APPENDIX E

Summary of 30 year Curb Ramp Program

**CITY OF THOUSAND OAKS
CURB RAMP PROGRAM - FY 2014-2044**

Description:
Construction of new curb ramps and reconstruction/modification of existing curb ramps in accordance with disability act (ADA) requirements on streets that are to be override Cape sealed and Micro surfaced as part of biennial street rehabilitation program and other locations determined by the disability access appeals and advisory board.

	YEAR																	
	2014 Completed			2016 Under Construction			2018			2020			2022			2024		
	ReConst.	New	Mod.	ReConst.	New	Mod.	ReConst.	New	Mod.	ReConst.	New	Mod.	ReConst.	New	Mod.	ReConst.	New	Mod.
NUMBER OF CURB RAMPS	193	38	300	67	1	329	112	41	48	112	41	48	112	41	48	112	41	48
ESTIMATED BUDGET	\$1,618,800 (Actual Cost)			\$321,600	\$4,800	\$329,000	\$571,200	\$209,100	\$52,800	\$604,800	\$221,400	\$57,600	\$638,400	\$233,700	\$62,400	\$672,000	\$246,000	\$67,200
				\$655,400			\$833,100			\$883,800			\$934,500			\$985,200		

	YEAR																	
	2026			2028			2030			2032			2034			2036		
	ReConst.	New	Mod.															
NUMBER OF CURB RAMPS	112	41	48	112	41	48	112	41	48	112	41	48	112	41	48	112	40	48
ESTIMATED BUDGET	\$705,600	\$258,300	\$72,000	\$739,200	\$270,600	\$76,800	\$772,800	\$282,900	\$81,600	\$817,600	\$299,300	\$86,400	\$862,400	\$315,700	\$91,200	\$907,200	\$324,000	\$96,000
	\$1,035,900			\$1,086,600			\$1,137,300			\$1,203,300			\$1,269,300			\$1,327,200		

	YEAR												Total			Total Number of Curb Ramps
	2038			2040			2042			2044			ReConst.	New	Mod.	
	ReConst.	New	Mod.													
NUMBER OF CURB RAMPS	112	40	48	112	40	48	112	40	48	112	40	48	1828	608	1301	3737
ESTIMATED BUDGET	\$952,000	\$340,000	\$100,800	\$996,800	\$356,000	\$105,600	\$1,052,800	\$376,000	\$110,400	\$1,108,000	\$396,000	\$115,200	\$18,980,000			
	\$1,392,800			\$1,458,400			\$1,539,200			\$1,619,200						

Assumptions:
2.5% inflation rate is assumed.

SUMMARY OF CURB RAMP PROGRAM (2018-2044)

	Reconstruct Curb Ramps	Modify Curb Ramps	New Curb Ramps
NUMBER OF CURB RAMPS	1568	672	570
ESTIMATED BUDGET (FY 2018-2044)	\$11,400,800	\$1,176,000	\$3,737,800
	\$16,314,600		

